AMERICAN SOCIETY OF HIGHWAY ENGINEERS

National Project of the Year Award

OFFICIAL ENTRY FORM

AWARD CATEGORY (Check One): ☐ Under $20 Million ☒ Over $20 Million

SPONSORING REGION (Check One):

☒ Northeast	☐ Great Lakes	☐ Northwest
☐ Mid-Atlantic	☐ North Central	☐ Rocky Mountain
☐ Southeast	☐ South Central	☐ Southwest

CONTACT INFORMATION FOR SUBMITTING REGION:

Contact Name: Scott R. Eshenaur ASHE Region Position: NPY Chairperson
Phone (Office): 717-790-9565 Phone (Mobile): 717-580-8426 E-Mail Address: sreshenaur@modjeski.com

PROJECT INFORMATION:

ENTERING AGENCY/COMPANY'S NAME: Stantec Consulting Services Inc.
PROJECT NAME: US Route 119 Youngwood Reconstruction TYPE: Highway Reconstruction & Traffic Calming
PROJECT LOCATION: US Route 119 through Youngwood Borough in Pennsylvania
CITY: Youngwood Borough COUNTY: Westmoreland STATE: Pennsylvania
FINAL CONSTRUCTION COST: $23,871,000 BUDGETED CONSTRUCTION COST: $21,906,000
PROJECT COMPLETION DATE: August 30, 2023

PROJECT ASHE SECTION: Southwest Penn ASHE SECTION CONTACT NAME: Amie S. Clawson, PE
PHONE (OFFICE): (412) 392-8730 PHONE (MOBILE): (412) 606-7928 E-MAIL: AClawson@saiengr.com

PROJECT TEAM:

PROJECT OWNER: Pennsylvania Department of Transportation, Engineering District 12-0
STREET ADDRESS: 825 North Gallatin Avenue Extension
CITY: Uniontown STATE: PA ZIP: 15401
CONTACT PERSON: Rachel D. Duda, PE PHONE: (724) 439-7259
E-MAIL ADDRESS: rduda@pa.gov

PROJECT DESIGN FIRM: Stantec Consulting Services Inc.
STREET ADDRESS: 750 Holiday Drive, Suite 220
CITY: Pittsburgh STATE: PA ZIP: 15220
CONTACT PERSON: Steve Moore, PE PHONE: (412) 219-5535
E-MAIL ADDRESS: Steve.Moore2@stantec.com

PRIME CONTRACTOR: Golden Triangle Construction Company
STREET ADDRESS: 8555 Old Steubenville Pike
CITY: Imperial STATE: PA ZIP: 15126
CONTACT PERSON: Tim Ryall PHONE: (724) 828-2800
E-MAIL ADDRESS: tryall@gtcpgh.com

Entry Form Completed By: Steve Moore, PE **Date:** January 25, 2024

STATEMENT OF COMMITMENT:

Stantec Consulting Services Inc. commits to having at least one representative from the project team in attendance at the awards luncheon.

VERIFICATION OF SUBSTANTIAL COMPLETION (1)

OS-2 (2-22)

pennsylvania
DEPARTMENT OF TRANSPORTATION

October 23, 2023

Golden Triangle Construction Company, Inc.
8555 Old Steubenville Pike
Imperial, PA 15126

Re: Westmoreland County
SR 0119 Section J20
US 119 Youngwood Recon
ECMS No.: 89191
Federal No.: T125-371-Z001

Colleagues:

The Department has scheduled a Semi-Final / After Action Review on the referenced project on Monday, November 13, 2023 at 9:30 AM.

The inspection party will meet at the project field office.

The Construction Project – Quality Survey for Design Items form must be completed in ECMS by the Contractor and IIC prior to the Semi Final / After Action Review. This form is now located in ECMS under the Closeout section of the Project Information screen.

Should you have any questions concerning this matter, you may contact me at 724-439-7286.

Sincerely,

Dominec A. Caruso, P.E.
Assistant Construction Engineer
Engineering District 12-0

120:DAC:jmk

cc: Richard Kercher, Federal Highway Administration
William L. Beaumariage, P.E., Assistant District Executive – Construction
Dominec A. Caruso, P.E., Assistant Construction Engineer
Huytu Nguyen, Inspector in Charge
James Sisul, Project Manager
David Forkey, Westmoreland County Maintenance Manager
Rachel D. Duda, P.E.
Kenneth A. Shimko, P.E.
Jeremy Hughes, P.E.
Roy A. Painter, P.E.

VERIFICATION OF SUBSTANTIAL COMPLETION (2)

ESTIMATE

Project: 89191 | **Standard / PENNDOT Oversight NHS** | **Post-Construction**

Short Description:	US 119 Youngwood Recon			**Org Code:**	1250
County:	Westmoreland	**SR:**	119	**Section:**	J20
District:	12	**Group ID:**	US 119 Youngwood Recon	**Municipality:**	YOUNGWOOD

Detail

Estimate:	079	**Status:**	Approved
Approved:	12/28/2023	**Payment Transaction:**	
Period Ending:	12/21/2023	**Project Total:**	$23,218,700.68
Payee:	GOLDEN TRIANGLE CONSTRUCTION COMPANY INC		
Address:	8555 OLD STEUBENVILLE PIKE IMPERIAL, PA 15126 -9194		

Summary

	Prior Estimates	This Estimate	Total	% of Project
Estimate Items:	$22,653,928.95	($104,938.60)	$22,548,990.35	97.08%
Adjustments:	$268,112.24	$104,938.60	$373,050.84	1.61%
Amount Tendered:	**$22,922,041.19**	**$0.00**	**$22,922,041.19**	**98.69%**

Payment Summary

Pay To Contractor: $0.00

Comments

Comments: No money due from the contractor on the estimate. The estimate is for work order purposes.

Adjustments

Adjustment	Reversal	Description	Item Number	Fund Code	Internal Order	Amount	Remaining
210	000	Estimate 079, FC=01, 12/21/2023		01	82630	$104,938.60	$104,938.60

Records 1 to 1 of 1 | Page 1 of 1 | Records Per Page: 500

Estimate Workflow

Status	Name	Disposition	Date/Time
Draft	Huytu Nguyen/PennDOT	Submit	12/21/2023 08:22:59 AM
ADE Review	Bill Beaumariage/PennDOT	Disapprove	12/21/2023 09:12:09 AM
As discussed with ACE this will be revised.			
Revise	Huytu Nguyen/PennDOT	Submit	12/21/2023 12:34:53 PM
This estimate is being processed due to item price re-negotiations. The negative is being held until the next estimate is processed which will result in a positive estimate.			
ADE Review	Bill Beaumariage/PennDOT	Approve	12/28/2023 08:20:08 AM

Payment Workflow

Project Narrative

US Route 119 Youngwood Reconstruction
SR 0119, Section J20

The State Route 0119, Section J20 (US Route 119 Youngwood Reconstruction) Project spans approximately 1.7 miles in length in Hempfield Township, New Stanton Borough, and Youngwood Borough in Westmoreland County, PA. The roadway serves as a major north/south connection between Interstates 70 and 76 to the south, and SR 30 and the City of Greensburg, the county seat, to the north. SR 119 is a 4-lane divided highway at the northern and southern extents of the project, transforming to a one-way pair with parking lanes as it traverses the heart of Youngwood Borough.

The corridor is an urbanized area that includes a mix of residential and commercial land use. A variety of transportation modes are present, including public transit, bicycle (Five Star Trail), and pedestrian. Originally constructed in the late 1930s and early 1940s, the roadway has suffered significant deterioration, fails to provide acceptable levels of service, and lacks attributes to maintain community and regional connectivity.

The purpose of the US Route 119 Youngwood Reconstruction Project was to address deficiencies, provide a reliable and efficient roadway that caters to the current and projected traffic, including pedestrians, and improve congestion to an acceptable level of service, while ensuring the traveling public has adequate safe access points throughout the corridor.

To achieve these goals, the project scope included total reconstruction of the roadway, including roadway drainage, sidewalks, ADA curb ramps, traffic signals, and removal of the existing box beam median barrier south of the Burton Avenue intersection and replacing it with a concrete median barrier. Numerous traffic calming and safety measures were implemented, including chicanes, curb extensions, narrowing lanes (road diet), placing optical speed bars, and installing radar speed display signing. Additionally, installation or reconstruction of sidewalks on two locally owned roadways provide cohesive pedestrian access routes from the residential land uses of Youngwood Borough to the parallel Five Star regional rail-trail located several blocks east of the project corridor.

SCORING CRITERIA RESPONSES:

1. Complexity

- Reconstructing the roadway and sidewalks while maintaining all modes of transportation was a particularly challenging aspect of the project. ADA accessible driveway transitions and ramps needed to be constructed while maintaining access, requiring dozens of pedestrian detours and complex staging of the work. Over five miles of sidewalk were constructed, as well as 32 intersections, eight traffic signals, 68 driveways and 200 ADA ramps. Also, close coordination with R. W. Sidley, Inc. concrete products was necessary as oversized loads (precast concrete culverts) are generated from their facility several times per week and traversed the project area.

- Utility involvement was a design element requiring intense focus. All gas, water and sanitary sewer lines were relocated or replaced throughout the project area. Over 350 new drainage inlets were installed and over 90 maintenance holes were relocated out of the wheel paths. PennDOT's share of the utility relocation costs exceeded $12 million.
- The complexity of the project is evident by the construction duration spanning nearly four complete construction seasons.

2. New Application of Existing Techniques / Originality / Innovation

- Traffic calming measures were implemented to reduce speeding and improve safety, including:
  - narrowing lanes from 12 feet to 11 feet (road diet),
  - installing multiple chicanes (lateral lane shifts) that passively reduce speed,
  - providing curb extensions at intersections to reduce pedestrian crossing times and improve safety by limiting the time a pedestrian is in a travel lane,
  - utilizing bulb-outs to define parking areas, passively reduce speeding, and eliminate the need for utility pole relocations,
  - installing optical speed bars and edge lines along curbs where not typically used, and
  - installing overhead radar speed display signing to notify motorists of their speed.
- High friction surface treatments were used to help reduce rear end collisions on approaches to an isolated signalized intersection.
- Thermoplastic pavement markings were used exclusively on the project (even for long lines) to improve visibility and reduce future maintenance.
- Despite adding lanes to improve capacity and adding sidewalks in areas where they were missing, the traffic calming features provided the opportunity to offset the additional impervious area with pervious areas, resulting in the elimination of the need for stormwater management facilities.
- Bulb-outs were used to limit temporary construction easements needed for driveway modifications at sensitive locations such as the Youngwood Post Office where driveway closure and property owner negotiations would be challenging.

3. Social / Economic Considerations

- The project addressed capacity issues by adding auxiliary turn lanes in select areas. This reduced congestion and made Youngwood Borough more attractive to both existing business patrons and future development as well. Even before construction ended, a national restaurant franchise began construction of a new facility on a parcel formerly used for truck parking.
- New sidewalks and traffic calming measures make Youngwood Borough a more walkable community and the Central Business District more attractive for businesses and patrons.
- Two locally owned roadways were resurfaced. Additionally, new sidewalks were installed and reconstructed in order to enhance and create pedestrian and bicycle routes from the residential areas of Youngwood Borough to the Five Star recreational trail that runs parallel to, and just east of, the US 119 project corridor.

- The project improvements help to create a sense of place for Youngwood Borough, which is bracketed by high-speed limited access divided highways.

4. Safety

- Safety for all modes of transportation was a primary project goal. Traffic calming measures were implemented to reduce speeding and improve safety, including:
  - narrowing lanes from 12 feet to 11 feet (road diet),
  - reducing the speed limit of the northbound lanes from 35 MPH to 25 MPH,
  - installing multiple chicanes (lateral lane shifts) that passively reduce speed,
  - providing curb extensions at intersections to reduce pedestrian crossing times and improve safety by limiting the time a pedestrian is in a travel lane,
  - utilizing bulb-outs to define parking areas, passively reduce speeding, and provide shelter for parked vehicles.
  - installing optical speed bars and edge lines along curbs where not typically used, and
  - installing overhead radar speed display signing to notify motorists of their speed.
- High friction surface treatments were used to help reduce rear end collisions on approaches to an isolated signalized intersection.
- Antiquated steel box beam median barrier was replaced with concrete glare screen at the southern end of the project for increased safety.
- Thermoplastic pavement markings were used exclusively on the project (even for long lines) to improve visibility.
- A private retail driveway was relocated to prevent wrong-way traffic maneuvers.

5. Aesthetics and Sustainable Features

- Youngwood Borough agreed to maintain the tree lawn (grass areas between curb sidewalk). Despite adding lanes to improve capacity and adding sidewalks in areas where they were missing, the traffic calming features provided the opportunity to offset the additional impervious area with pervious areas.
- These enhancements prevented the need for stormwater control measures as there was not a net increase in impervious area.
- The project improvements help to create a sense of place for the Borough, which is bracketed by high-speed limited access divided highways.
- New sidewalks were installed or reconstructed along Depot and Hillis Streets in order to enhance and create pedestrian and bicycle routes from the residential areas of Youngwood Borough to the Five Star recreational trail that runs parallel to and just east of the US 119 project corridor.
- Thermoplastic pavement markings were used exclusively on the project (even for long lines) to reduce future maintenance.

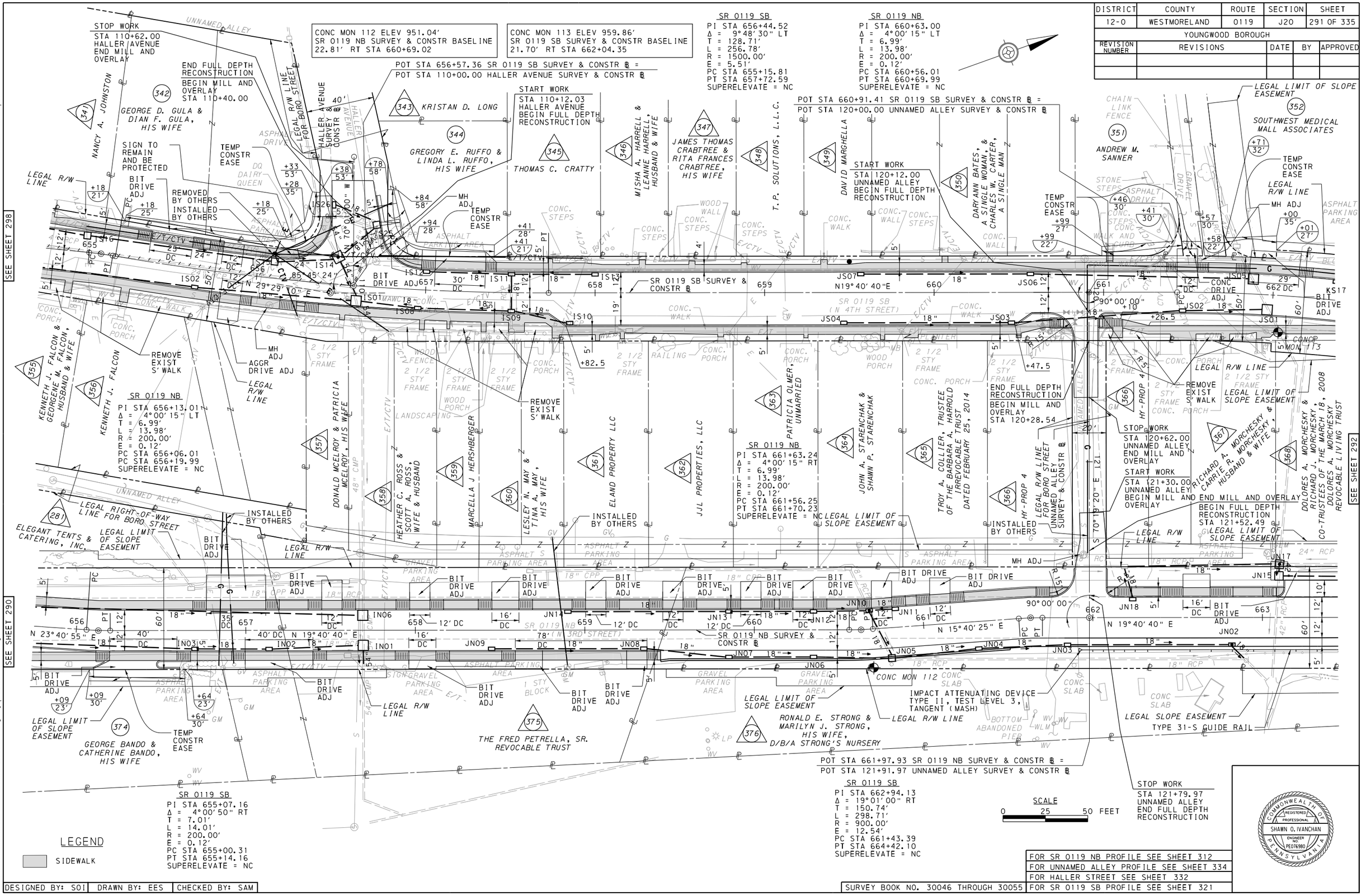
6. Meeting or Exceeding Owner's / Client's Needs

- Throughout design a list of "hot items" was provided to PennDOT to highlight issues that required their decision or input in order to maintain the aggressive schedule.
- Despite adding lanes to improve capacity and adding sidewalks in areas where they were missing, the traffic calming features provided the opportunity to offset the additional impervious area with pervious areas, resulting in the elimination of the need for stormwater management facilities.
- The final construction cost **($23,871,000)** was within 9% of the pre-bid estimate **($21,906,000)** despite being bid during the first month of the pandemic and unprecedented construction inflation.
- Perhaps the best measure of how well the solution met the owner's goals are the three "Consistently Exceeds Expectations" evaluations received for the Preliminary Engineering, Final Design and Right of Way acquisition stages of the project. The evaluation for services during construction is not yet available.
- The design and construction schedules were both met. Preliminary Engineering was completed one week in advance of the 10 months required. Right of Way Acquisition (73 claims) was completed in 11 months and Final Design was accelerated by one week within the last month of Final Design to accommodate a revised client letting schedule.

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
12-0	WESTMORELAND	0119	J20	291 OF 335
	YOUNGWOOD BOROUGH			

REVISION NUMBER	REVISIONS	DATE	BY	APPROVED

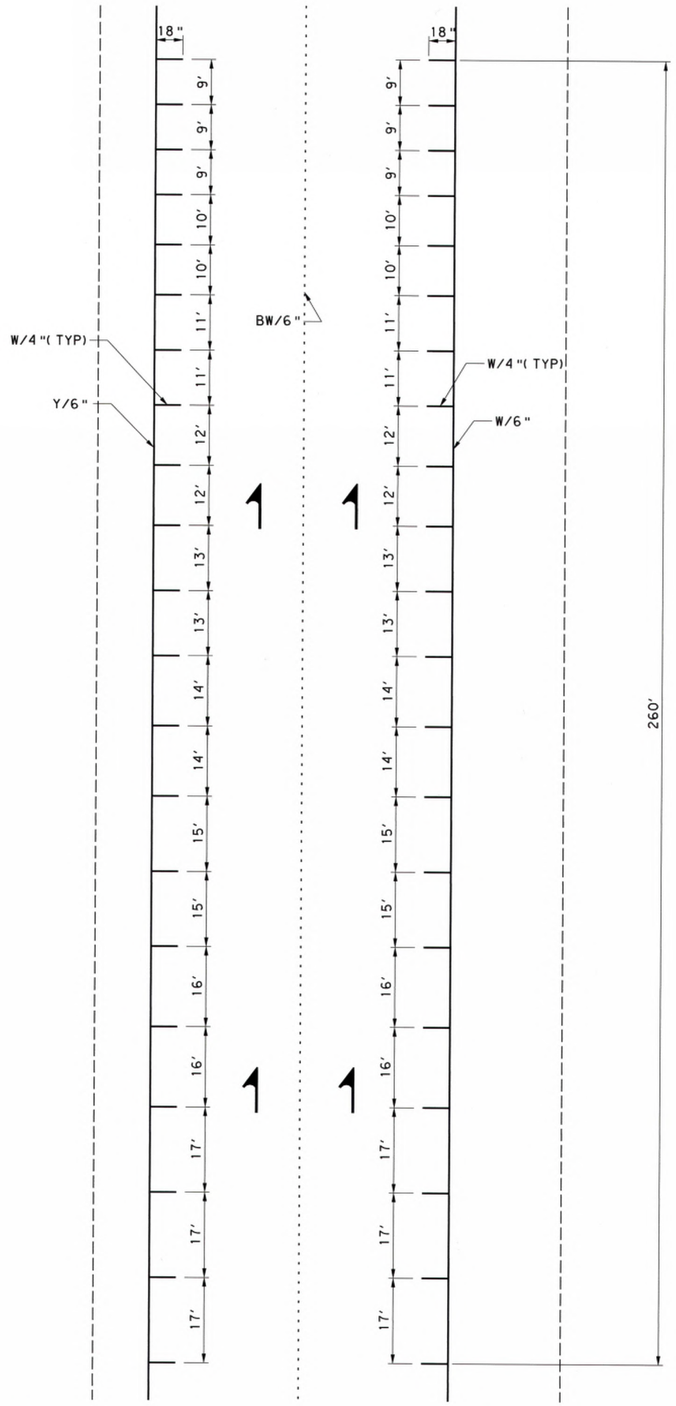
DOT_2015
1534_CN_Blank_Color_249-254.tbl
DOT_ANSI D_PDF.plt



2/12/2020
WORKSPACE: DOT_2015 (SURVEY FEET)
U:\2027131534\1.100 Records\13.0 CADD\13.1 Highway\prCN1534cnPS10.dgn

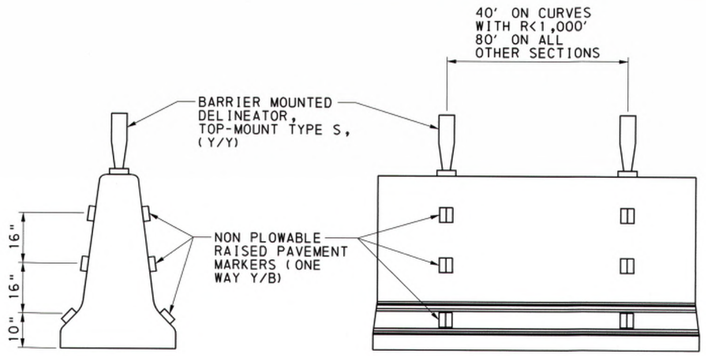
DISTRICT	COUNTY	ROUTE	SECTION	SHEET
12-0	WESTMORELAND	0119	J20	1 OF 36
YOUNGWOOD & NEW STANTON BOROS, HEMPFIELD TOWNSHIP				

REVISION NUMBER	REVISIONS	DATE	BY



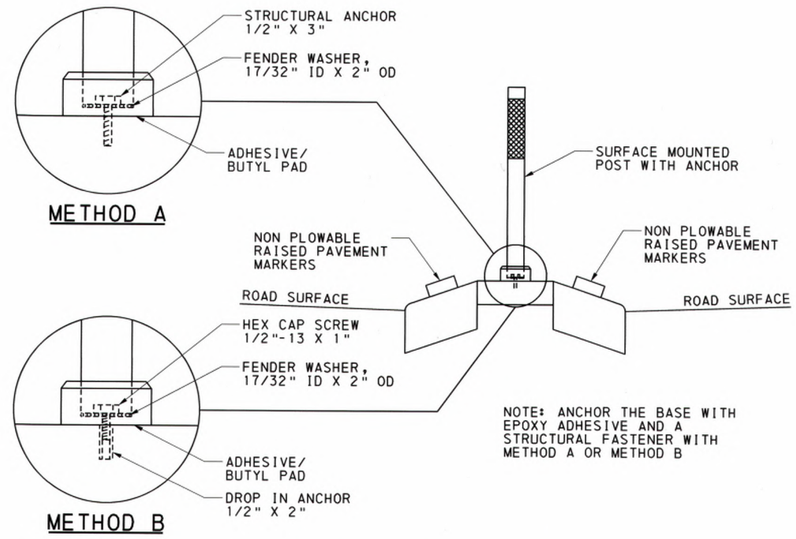
OPTICAL SPEED BAR DETAIL (45 MPH TO 25 MPH)

STA 601+00 TO STA 603+60 SR 0119 NB
STA 670+40 TO STA 673+00 SR 0119 SB
NOT TO SCALE



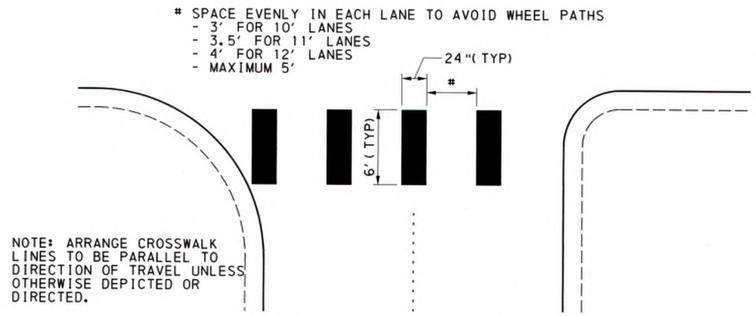
CONCRETE GLARE SCREEN MEDIAN BARRIER DELINEATION DETAIL

NOT TO SCALE



ISLAND DELINEATION DETAIL

NOT TO SCALE



CROSSWALK MARKING DETAIL

NOT TO SCALE

GENERAL NOTES

1. INSTALL PAVEMENT MARKINGS AND DELINEATION IN ACCORDANCE WITH THE DETAILS IN THESE DRAWINGS AND THE FOLLOWING, OR AS DIRECTED BY PENNDOT DISTRICT 12-0 TRAFFIC ENGINEERING REPRESENTATIVE.
   - A. PUBLICATION 408, "SPECIFICATIONS", 2016, CHANGE 7.
   - B. FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 2009 AND CURRENT REVISIONS.
   - C. PENNDOT PUBLICATION 212, "OFFICIAL TRAFFIC CONTROL DEVICES", MARCH 2006.
   - D. PENNDOT PUBLICATION 111, "TRAFFIC CONTROL-PAVEMENT MARKINGS AND SIGNING STANDARDS", JUNE 2013.
   - E. PENNDOT PUBLICATION 46, "TRAFFIC ENGINEERING MANUAL", FEBRUARY 2012, CHANGE 1 (MARCH 2014).
2. DETAILS OTHER THAN THOSE INDICATED ARE ON THE FOLLOWING STANDARD DRAWINGS:

TC-8600	13 SHEETS	JUNE 13, 2013
TC-8602	4 SHEETS	JUNE 13, 2013
TC-8604	4 SHEETS	JUNE 13, 2013

3. APPLY ALL PAVEMENT MARKINGS AT THE WIDTH INDICATED.
4. ALL PAVEMENT MARKING LINES AND LEGENDS SHALL BE DURABLE LONG-LIFE MATERIAL. ON ASPHALT SURFACES, USE HOT THERMOPLASTIC AND ON CONCRETE SURFACES, USE EPOXY.
5. THE CONTRACTOR SHALL REAPPLY ANY EXISTING PAVEMENT MARKING THAT IS DAMAGED WITHIN OR BEYOND THE START/STOP OF WORK DUE TO CONSTRUCTION ACTIVITY AT NO ADDITIONAL COST TO THE DEPARTMENT.
6. INSTALL RAISED PAVEMENT MARKERS IN ACCORDANCE WITH PENNDOT PUBLICATION 111 TRAFFIC CONTROL.
7. INSTALL TYPE B AND D DELINEATOR TO ALL GUIDE RAIL WITHIN PLAN LIMITS AT INTERVALS SPECIFIED PER PUB 111, TC- 8604, OR AS DIRECTED BY PENNDOT DISTRICT 12-0 TRAFFIC ENGINEERING REPRESENTATIVE.
8. INSTALL NON PLOWABLE RAISED PAVEMENT MARKERS TO ALL NEWLY INSTALLED/EXISTING CONCRETE MOUNTABLE CURB AND CONCRETE BARRIER WITHIN PLAN LIMITS AT INTERVALS SPECIFIED PER PUB 111, TC-8604, OR AS DIRECTED BY PENNDOT DISTRICT 12-0 TRAFFIC ENGINEERING REPRESENTATIVE.
9. REFER TO THE TRAFFIC SIGNAL PLANS FOR ADDITIONAL SIGNS AND PAVEMENT MARKINGS ASSOCIATED WITH A TRAFFIC SIGNAL.
10. APPLY ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS
11. THE CONTRACTOR SHALL REAPPLY ANY EXISTING PAVEMENT MARKING THAT IS DAMAGED WITHIN OR BEYOND THE START/STOP OR WORK DUE TO CONSTRUCTION ACTIVITY AT NO ADDITIONAL COST TO THE DEPARTMENT
12. INSTALL TYPE B AND D DELINEATORS TO ALL GUIDE RAIL WITHIN PLAN LIMITS AT INTERVALS SPECIFIED PER PUBLICATION 111, TC-8604, OR AS DIRECTED BY PA. DEPARTMENT OF TRANSPORTATION
13. INSTALL FLEXIBLE DELINEATOR POSTS ON NEWLY INSTALLED ISLANDS, WITHIN PLAN LIMITS, IN ACCORDANCE WITH THE PLAN DETAIL AND DRAWING

SHEET INDEX BLOCK

COMMONWEALTH OF PENNSYLVANIA
REGISTERED PROFESSIONAL
SHAWN O. IVANCHAK
ENGINEER NO. PE076980
02/04/20

PAVEMENT MARKING PLAN

DESIGNED BY: WAB	DRAWN BY: ZPS	CHECKED BY: SAM

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
12-0	WESTMORELAND	0119	J20 R/W	30 OF 104
YOUNGWOOD BOROUGH				

REVISION NUMBER	REVISIONS	DATE	BY	APPROVED

BM 3 ELEV 981.92'
SR 0119 SB SURVEY & R/W BASELINE
14' LT STA 605+23
BENCH TIE IN UN-NUMBERED POLE

FOR PARCEL INFORMATION	SEE SHEET
(42)	71
(46)	73
(48)	74
(43)	72
(32) (35) (37)	95
(44) (47) (85)	96

LEGEND

- REQUIRED RIGHT-OF-WAY AREA
- REQUIRED RIGHT-OF-WAY AREA FOR BORO STREET
- SIDEWALK
- MAST ARM
- SIGNAL PEDESTAL
- GW — GUY WIRE

POT STA 609+62.73 SR 0119 SB SURVEY & R/W ℄ =
POT STA 2+00.00 HILLIS STREET SURVEY & R/W ℄

SR 0119 NB
HORIZONTAL CURVE DATA
PI STA 611+24.34
Δ = 30°56'00" LT
T = 262.86'
L = 512.89'
R = 950.00'
E = 35.70'
PC STA 608+61.47
PT STA 613+74.37
SUPERELEVATE = NC

SR 0119 SB
HORIZONTAL CURVE DATA
PI STA 607+55.97
Δ = 16°33'00" LT
T = 77.81'
L = 154.54'
R = 535.00'
E = 5.63'
PC STA 606+78.16
PT STA 608+32.69
SUPERELEVATE = NC

SR 0119 NB
HORIZONTAL CURVE DATA
PI STA 605+76.93
Δ = 10°37'08" RT
T = 102.23'
L = 203.87'
R = 1100.00'
E = 4.74'
PC STA 604+74.70
PT STA 606+78.57
SUPERELEVATE = NC

POC STA 610+23.38 SR 0119 NB SURVEY & R/W ℄ =
POT STA 4+03.18 HILLIS STREET SURVEY & R/W ℄ BK =
POT STA 10+00.00 HILLIS STREET SURVEY & R/W ℄ AHD

BEGIN AUTHORIZATION
STA 2+28.67
HILLIS STREET

END AUTHORIZATION
STA 606+12.00
SEG 0191 OFFSET 0223
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 606+03.00
SEG 0191 OFFSET 0214
SR 0119 SEC J20 R/W

END AUTHORIZATION
STA 608+02.00
SEG 0191 OFFSET 0413
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 607+85.00
SEG 0191 OFFSET 0396
SR 0119 SEC J20 R/W

END AUTHORIZATION
STA 610+40.00
SEG 0191 OFFSET 0651
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 611+53.00
SEG 0191 OFFSET 0764
SR 0119 SEC J20 R/W

END AUTHORIZATION
STA 612+00.00
SEG 0191 OFFSET 0811
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 3+44.00
HILLIS STREET

END AUTHORIZATION
STA 3+78.00
HILLIS STREET

END AUTHORIZATION
STA 3+06.00
HILLIS STREET

BEGIN AUTHORIZATION
STA 609+30.00
SEG 0191 OFFSET 0541
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 607+14.00
SEG 0190 OFFSET 0317
SR 0119 SEC J20 R/W

END AUTHORIZATION
STA 607+27.00
SEG 0190 OFFSET 0330
SR 0119 SEC J20 R/W

BEGIN AUTHORIZATION
STA 609+50.00
SEG 0190 OFFSET 0553
SR 0119 SEC J20 R/W

END AUTHORIZATION
STA 610+97.00
SEG 0190 OFFSET 0700
SR 0119 SEC J20 R/W

TEMPORARY CONSTRUCTION EASEMENT
REQUIRED RIGHT-OF-WAY LINE
REQUIRED SIDEWALK EASEMENT
HEDGES TO REMAIN AND BE PROTECTED
LEGAL RIGHT-OF-WAY LINE
LEGAL SLOPE EASEMENT
LEGAL RIGHT-OF-WAY LINE FOR BORO STREET
REQUIRED RIGHT-OF-WAY LINE FOR BORO STREET
REMOVED BY OTHERS
INSTALLED BY OTHERS
STEPS AND WALKWAY TO REMAIN AND BE PROTECTED
WELL TO REMAIN AND BE PROTECTED
SIGN TO REMAIN AND BE PROTECTED
REMOVED BY OTHERS RIGHT-OF-WAY RESERVED BY MAWC +30
BIT DRIVE ADJ
GRAVEL DRIVE ADJ
EXISTING RIGHT-OF-WAY OF MAWC

(32) DALE L. MENSCH & DARCIE J. MENSCH, HUSBAND & WIFE
(33) MICHAEL F. HAIGH & CHERYL JONES HAIGH, HIS WIFE
(34) JAMES E. CRAIG, JR. & RICHARD L. CRAIG
(35) CYNTHIA M. HRIBAL
(36) EDWARD J. IMMEL & DENNIS
(37) PAIGE M. ELINSKY
(38) J. WILKINS & J. WILKINS, WIFE & WESLEY RHONDA & HUSBAND
(84) THOMAS C. SHIRER, JR. & SHIRER, HIS WIFE
(85) ANTHONY L. BOMPIANI & BOMPIANI, HIS WIFE
(41) WASHINGTON STREET PROPERTY MANAGEMENT, LLC
(42) TINA K. PERSON & PEORIA, A SINGLE PERSON & LISA M. PEORIA, A SINGLE PERSON
(43) 7-ELEVEN, INC.
(44) REGIS A. KLINCHOCK & ANNABELLE KLINCHOCK, HIS WIFE
(46) BOROUGH OF YOUNGWOOD
(47) STELLA MORGAN, MARRIED
(48) BRUCETON FARM SERVICE, INC.
(122) ANTHONY L. BOMPIANI & BOMPIANI, HIS WIFE & RENEE

THIS PLAN IS NOT TO BE SUBSTITUTED FOR A BOUNDARY SURVEY.

REQUIRED RIGHT-OF-WAY FOR BOROUGH STREET. RIGHT-OF-WAY ACQUIRED FOR THE BENEFIT OF THE APPLICABLE MUNICIPALITY IN THE SAME INTEREST AS DESIGNED HEREIN FOR REQUIRED RIGHT-OF-WAY FOR STATE HIGHWAY PURPOSES, ALONG WITH A TEMPORARY CONSTRUCTION EASEMENT FOR THE BENEFIT AND USE OF THE COMMONWEALTH TITLE SHALL VEST IN THE APPLICABLE LOCAL MUNICIPALITY UPON ACQUISITION AND MAINTENANCE RESPONSIBILITY SHALL TRANSFER UPON THE COMPLETION OF CONSTRUCTION.

SIDEWALK EASEMENT. AN EASEMENT FOR THE CONSTRUCTION, INSPECTION, MAINTENANCE, REPAIR, RECONSTRUCTION AND ALTERATION OF A SIDEWALK. THE EASEMENT SHALL NOT PREVENT THE PROPERTY OWNER FROM MAKING ANY LEGAL USE OF THE AREA WHICH IS NOT DETRIMENTAL TO ITS USE FOR SIDEWALK PURPOSES.

TEMPORARY CONSTRUCTION EASEMENT. AN EASEMENT TO USE THE LAND AS NECESSARY DURING CONSTRUCTION OF THE PROJECT. THE EASEMENT IS REQUIRED ONLY UNTIL THE CONSTRUCTION OR WORK INDICATED BY THE PLAN IS COMPLETED, UNLESS SOONER RELINQUISHED IN WRITING BY THE DEPARTMENT.

BUILDINGS AND STRUCTURES MARKED [0] ARE ENCROACHMENTS WHICH ARE THE RESPONSIBILITY OF THE PROPERTY OWNERS TO REMOVE.

SCALE 0 25 50 FEET

COMMONWEALTH OF PENNSYLVANIA — REGISTERED PROFESSIONAL — BRODY M. TUNNO — LAND SURVEYOR — SU075462

COMMONWEALTH OF PENNSYLVANIA — REGISTERED PROFESSIONAL — SHAWN O. IVANCHAN — ENGINEER NO. PE076980

SEE SHEET 29
SEE SHEET 39
SEE SHEET 31
SEE SHEET 45
FOR PROFILE, SEE SHEETS 50, 59 &68

DESIGNED BY: SOI | DRAWN BY: EES | CHECKED BY: MEG

SURVEY BOOK NO. 30046 THROUGH 30055

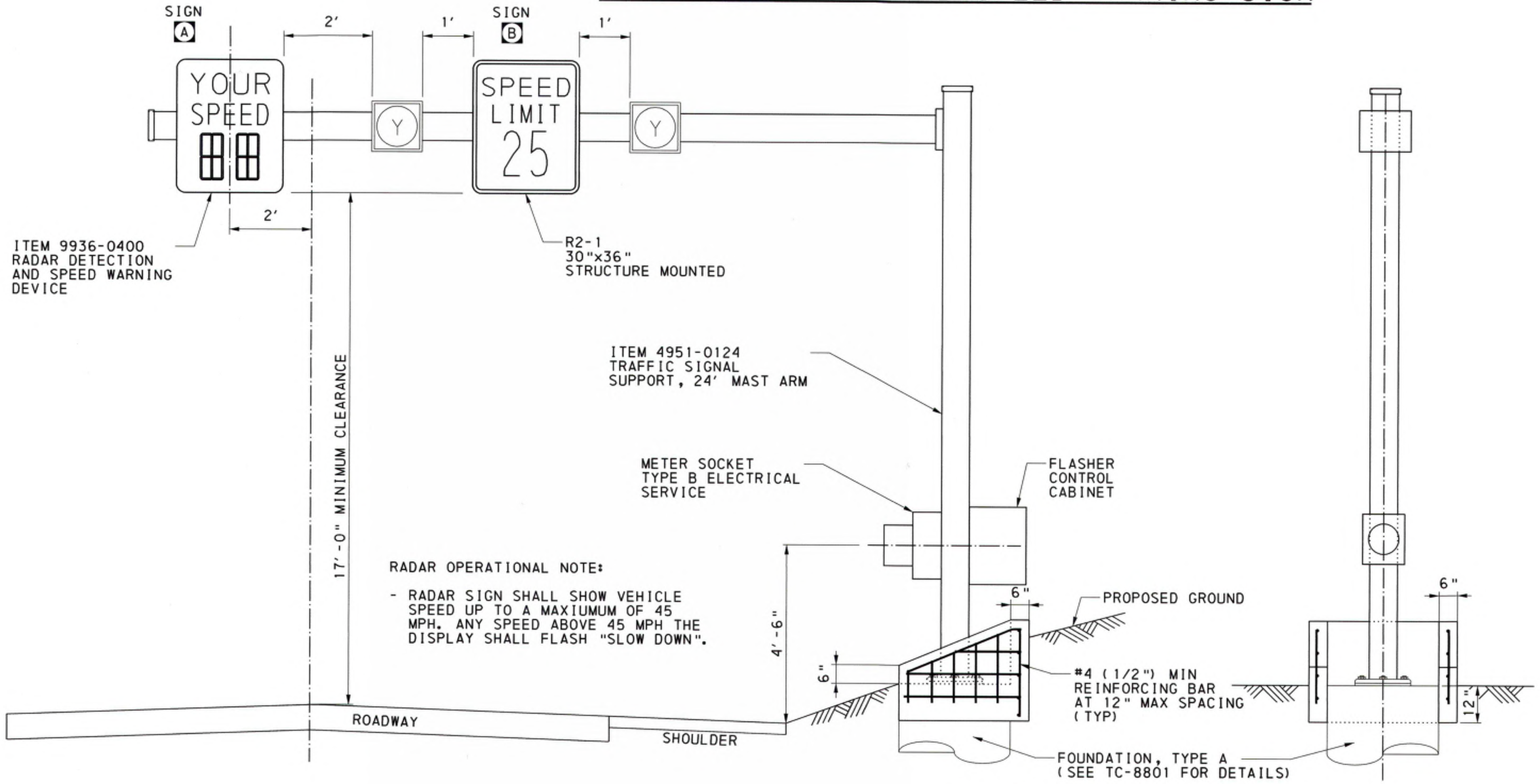
DOT_2015
1534_RW_Blopk_Color_249-254.tbl
DOT_ANSI D_PDF_plt
5/29/2019
WORKSPACE: DOT_2006 (SURVEY FEET)
U:\2027131\534\1.100 Records\13.0 CADD\13.1 Highway\pr\RW\1534rwPS03.dgn

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
12-0	WESTMORELAND	0119	J20	30 OF 31

YOUNGWOOD BOROUGH

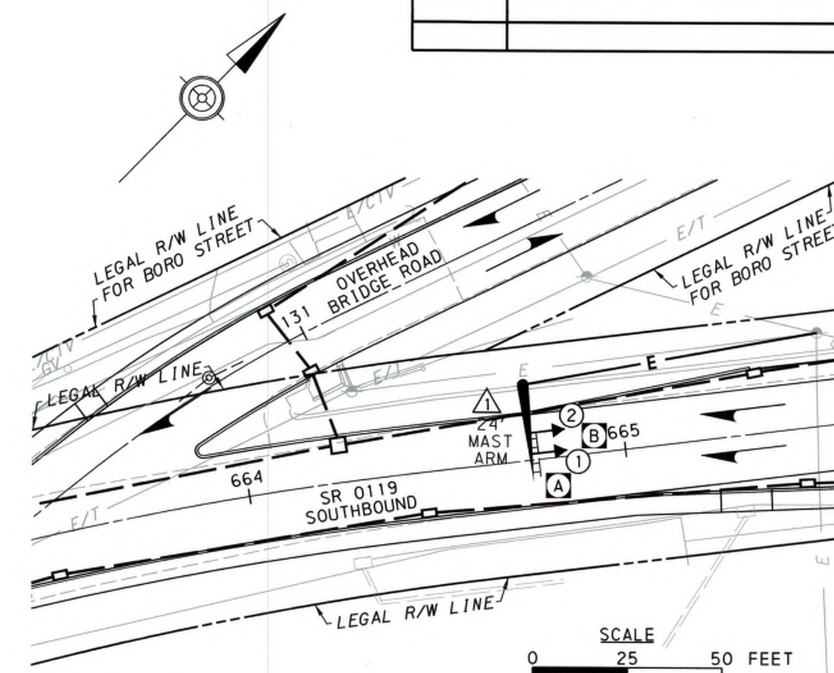
REVISION NUMBER	REVISIONS	DATE	BY

FLASHING WARNING DEVICE PLAN
RADAR DETECTION AND SPEED WARNING SIGN



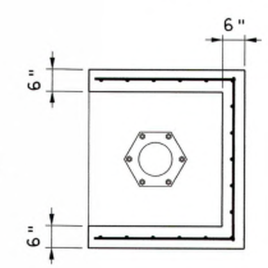
RADAR OPERATIONAL NOTE:

- RADAR SIGN SHALL SHOW VEHICLE SPEED UP TO A MAXIUMUM OF 45 MPH. ANY SPEED ABOVE 45 MPH THE DISPLAY SHALL FLASH "SLOW DOWN".



LEGEND

- A — STRUCTURE MOUNTED SIGN
- ⚠1 — TRAFFIC SIGNAL SUPPORT, 24' MAST ARM
- TRAFFIC FLOW ARROW



RADAR SPEED DISPLAY SIGN DETAIL
(NOT TO SCALE)

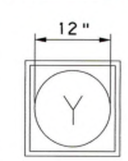
FOUNDATION NOTES:

- INSTALLATION OF WALL SHALL BE AS DIRECTED DEPENDANT UPON FINAL GRADING.
- PROVIDE 1 ½" MINIMUM CONCRETE COVER.
- DIMENSIONS SHOWN ARE MINIMUM.

MISCELLANEOUS

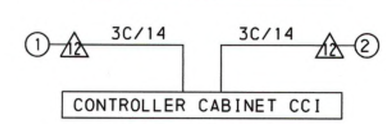
ITEM NUMBER / UNIT	DESCRIPTION	QUAN.	LOCATION	REMARKS
0936 0300 / EA	INTERNALLY ILLUMINATED SIGN	1	⚠1 B	
9000 0029 / EA	RADAR DETECTION AND SPEED WARNING DEVICE	1	⚠1 A	
0952 2052 / EA	CONTROLLER ASSEMBLY, SOLID STATE FLASHER, DUAL CIRCUIT, TYPE II MOUNTING	1	CCI	
0954 0201 / LF	SIGNAL CABLE, 14 AWG, 3 CONDUCTOR	91	CCI TO ①②	
4954 0402 / EA	ELECTRICAL SERVICE, TYPE B, WITH UPGRADED SURGE PROTECTION	1	⚠1	
0955 3206 / EA	VEHICULAR SIGNAL HEAD, ONE 12" SECTION	2	①②	

SIGNALS



1 AND 2

WIRING DIAGRAM



- 3C/14 CABLE (NO. OF CONDUCTORS)/SIZE (AWG)
- ⚠1 SIGNAL SUPPORT
- ① SIGNAL HEAD
- CCI FLASHER CONTROLLER CABINET

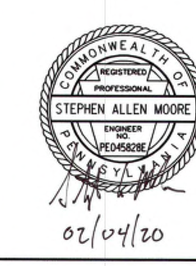
SIGNS (FOR INFORMATION ONLY)

SYMBOL	NOMENCLATURE	DESCRIPTION	SIZE	QTY
A	S-1	RADAR DETECTION AND SPEED WARNING DEVICE	30"X36"	1
B	R2-1	SPEED LIMIT (25) (INTERNALLY ILLUMINATED)	30"X36"	1

TRAFFIC SIGNAL SUPPORTS*

STRUCTURE NUMBER	ITEM NUMBER / UNIT	DESCRIPTION	QUAN.	ROUTE	STA.	SIDE	OFFSET	MAST ARM H	K	ARM "A" L	SIGNAL LOCATION M	N	O	P	SIGN LOCATION R	S	T	U	Y	W
⚠1	4951 0124 / EA	TRAFFIC SIGNAL SUPPORT, 24' MAST ARM, WITH SPECIAL FOUNDATION	1	SR 0119	664+75	LT	20.0	17	10	24	17.0	11.0			20.5 **	14.0 ***				

* REFER TO TRAFFIC STANDARDS - SIGNALS, TC-8801, FOR LETTER DESIGNATIONS.
** RADAR DETECTION AND SPEED DISPLAY SIGN
*** INTERNALLY ILLUMINATED SIGN



02/04/20

COUNTY: WESTMORELAND
MUNICIPALITY: YOUNGWOOD BOROUGH
INTERSECTION OF: S.R. 0119 SB (4TH ST), SEGMENT 0211 OFFSET 1397

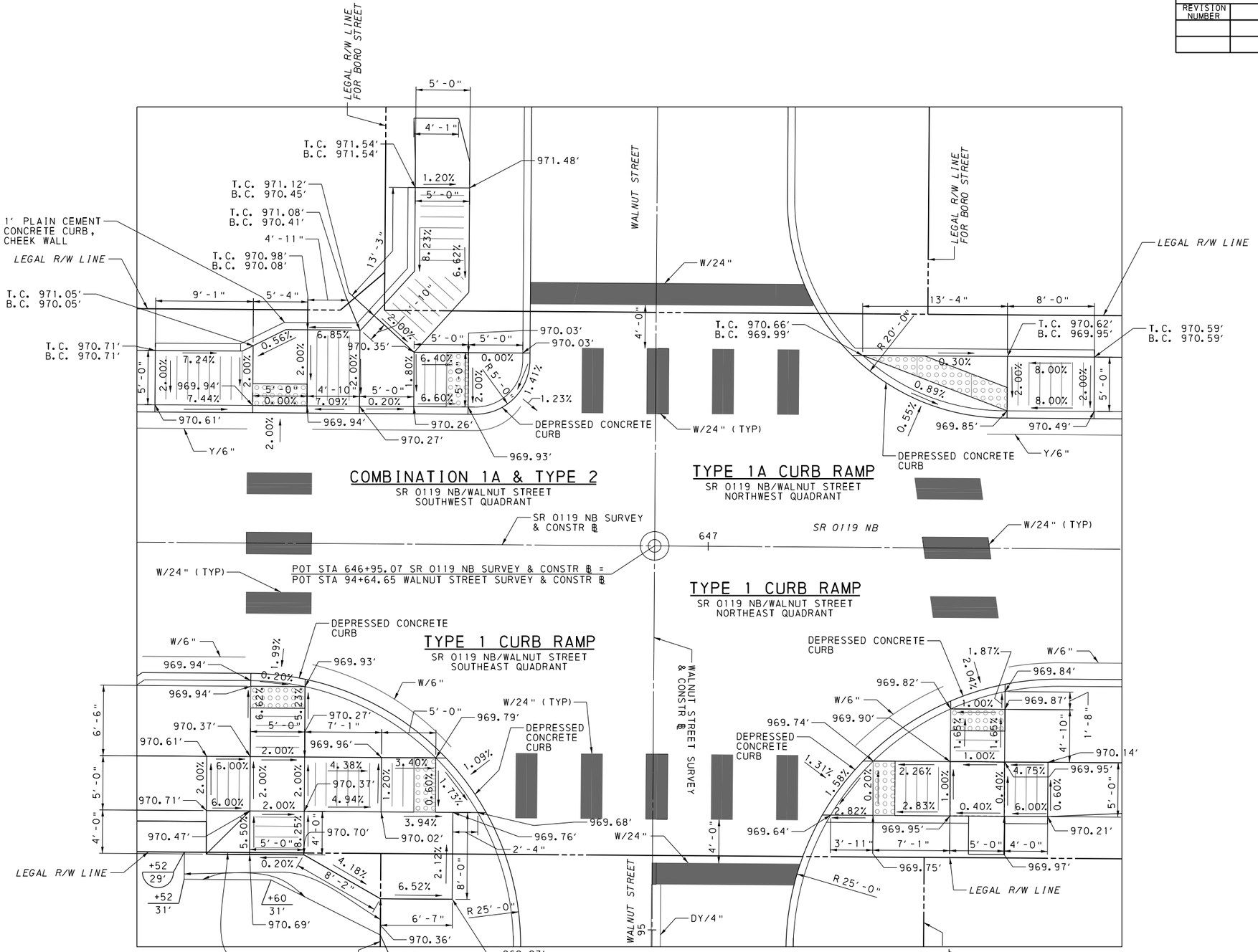
RECOMMENDED:

DISTRICT TRAFFIC ENGINEER DATE

DOT_2015
1534_TS_Black_Color_249-254.tbl
DOT_ANSI_D_PDF.plt
2/4/2020
WORKSPACE: DOT_2006 (SURVEY FEET)
U:\2027\1534\1.100 Records\13.0 CADD\13.3 Traffic\prSG\1534sg_RSD01.dgn

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
12-0	WESTMORELAND	0119	J20	59 OF 335
YOUNGWOOD BOROUGH				

REVISION NUMBER	REVISIONS	DATE	BY	APPROVED



LEGEND

DETECTABLE WARNING SURFACE

NOTES

1. CONSTRUCT CURB RAMPS AS PER RC-67M.
2. ALL ELEVATIONS RELATIVE TO TOP OF CURB UNLESS NOTED

SCALE

0 5 10 FEET

CURB RAMP DETAILS

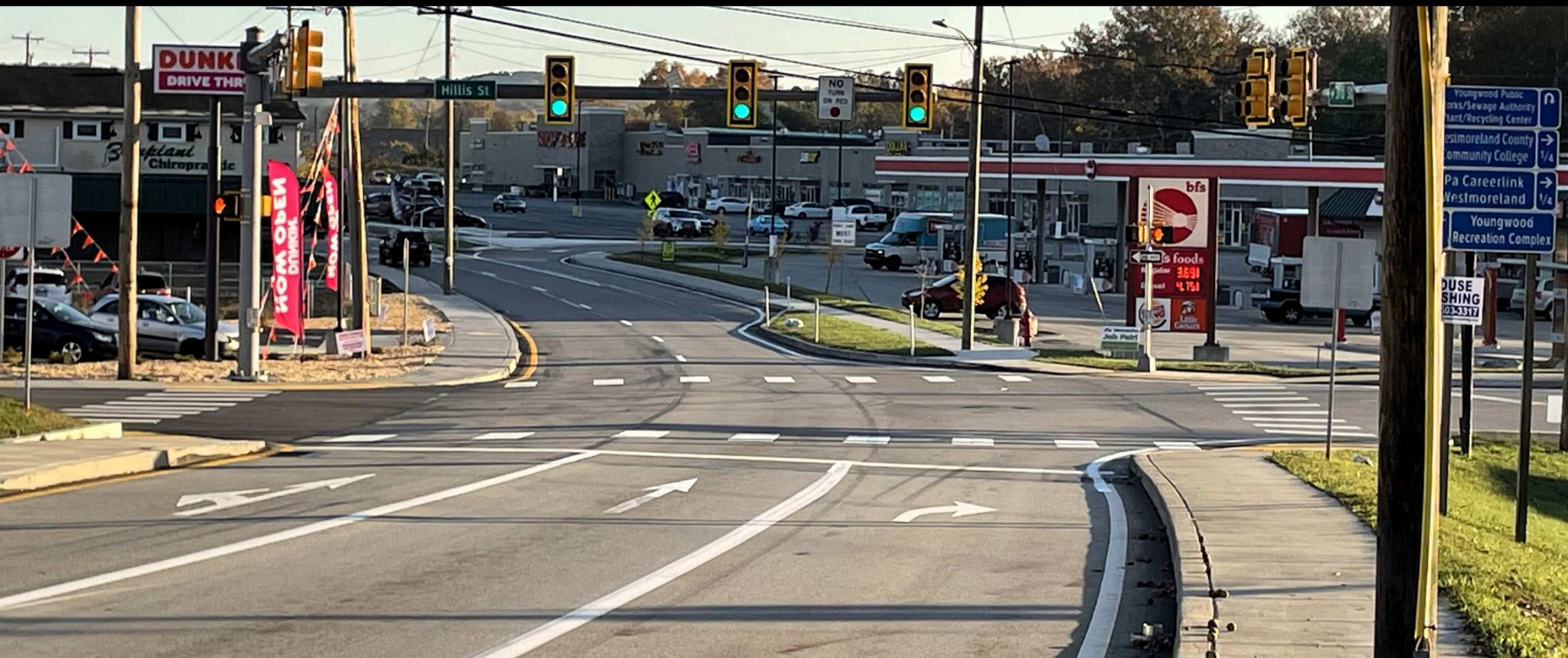
COMMONWEALTH OF PENNSYLVANIA
REGISTERED PROFESSIONAL
SHAWN O. IVANCHAN
ENGINEER NO. PE076980

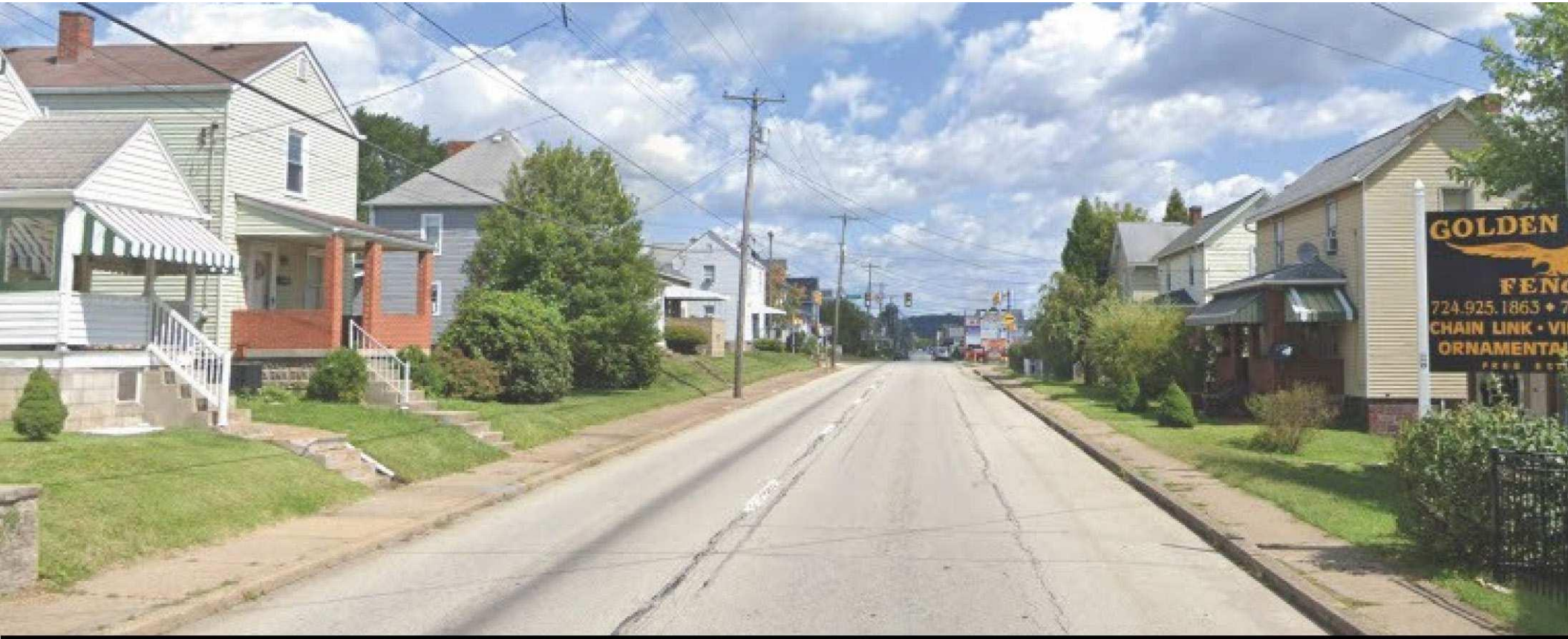
DESIGNED BY: SOI | DRAWN BY: EES | CHECKED BY: SAM

DOT_2015
1534_CN_Blank_Color_249-254.tbl
DOT_ANSI D_PDF.plt

2/6/2020
WORKSPACE: DOT_2015 (SURVEY FEET)
U:\2021\1534\1\100 Records\13.0 CADD\13.1 Highway\prCN\1534crDT19.dgn


Hillis St
DO NOT ENTER
STOP
DO NOT ENTER
NO TURN ON RED
bfs foods
Little Caesars
Pa Careerlink Westmoreland
Youngwood Recreation Complex


Hillis St
NO TURN ON RED
bfs
Youngwood Public Works/Sewage Authority Plant/Recycling Center
Pa Careerlink Westmoreland
Youngwood Recreation Complex


GOLDEN
724.925.1863


724.925.1863


SPEED
LIMIT
25


MONDAY NOON
WESTMORELAND
COMMUNITY COLLEGE